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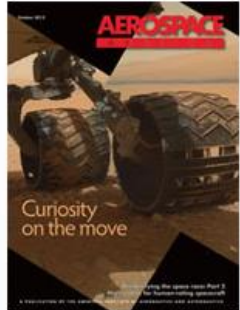
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Analysis of Different Approaches to Modeling Nozzle Flows in the Near Continuum Regime
[Deborah Levin](#), [Evgeny Titov](#), [Sergey Gimelshein](#), [Natalia Gimelshein](#)
[46th AIAA Aerospace Sciences Meeting and Exhibit](#), 2008, 10.2514/6.2008-750
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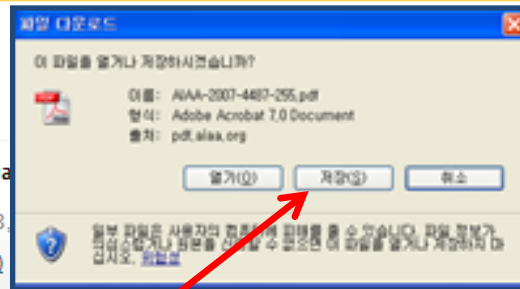
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37th AIAA Fluid Dynamics Conference and Exhibit
25 - 28 June 2007, Miami, FL

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Analysis of Crossflow Transition Flight Experiment aboard the Pegasus Launch Vehicle

Mujeeb R. Malik^{*}
NASA Langley Research Center, Hampton, VA 23681

Fei Li[†]
National Institute of Aerospace, Hampton, VA 23666

and

Meelan Choudhari[‡]
NASA Langley Research Center, Hampton, VA 23681

The Pegasus wing-glove flight experiment¹⁻⁴ was designed to provide crossflow transition data at high Mach numbers, specifically to help validate stability based predictions for transition onset in a flight environment. This paper provides an analysis of the flight experiment, with emphasis on computational results for crossflow disturbances and the correlation of disturbance growth factors with in-flight transition locations via the e^N method. Implications of the flight data for attachment line stability are also examined. Analysis of the thermocouple data reveals that transition (from turbulent to laminar flow) was first detected during the ascending flight of the rocket when the free stream Mach number exceeded about 4. Therefore, computations have been performed for flight Mach numbers of 4.13, 4.35, 4.56 and 4.99. Due to continually decreasing unit Reynolds number at higher altitudes, the entire wing-glove boundary layer became laminar at the highest flight Mach number computed above. In contrast, the boundary layer flow over the inboard tile

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